

# LOCAL REVIEW BODY

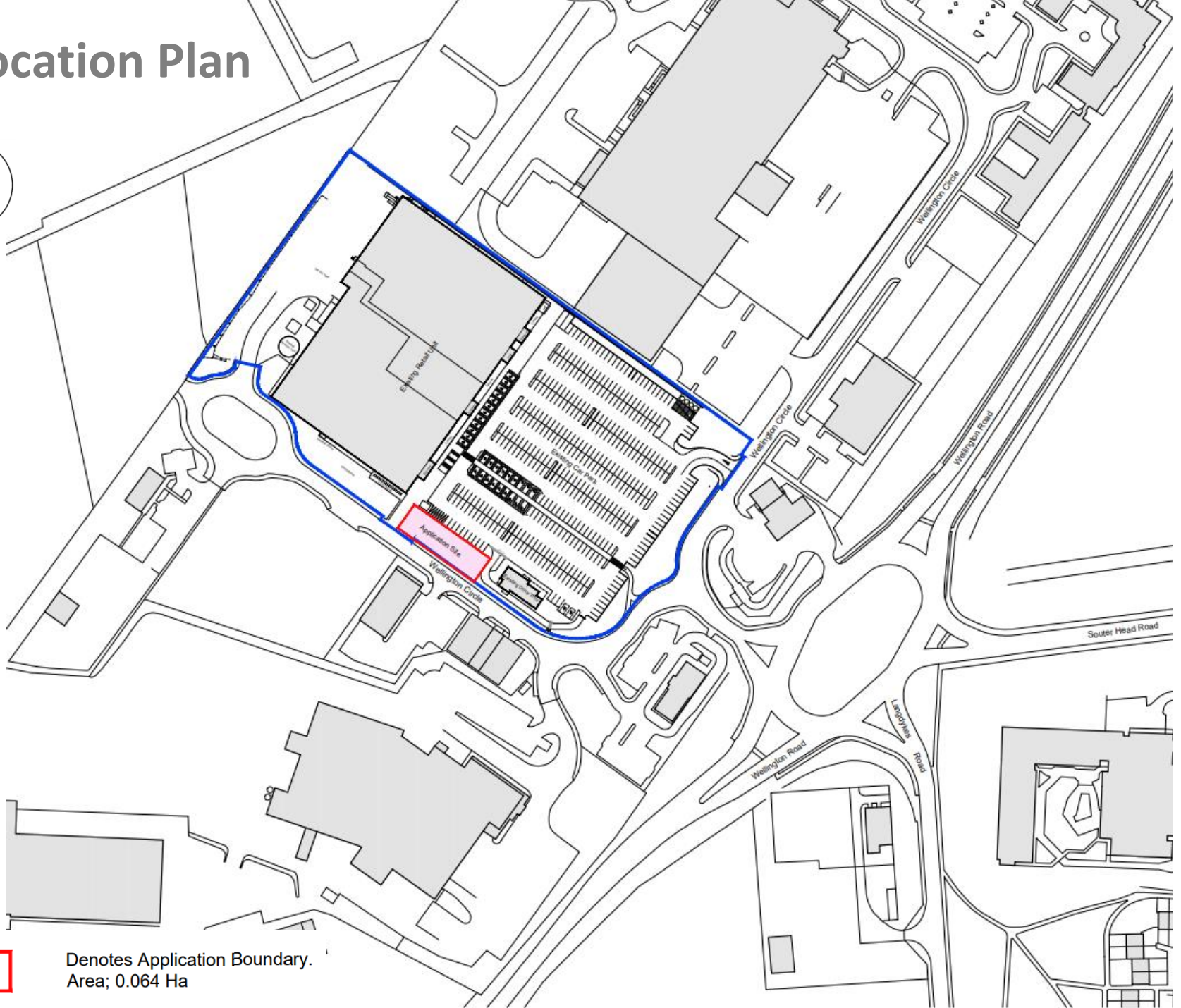
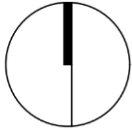


191588/DPP– Review against refusal of planning permission for:

Erection of two (Class 1) retail units

At: Land to East of Ikea, Wellington Circle, Aberdeen

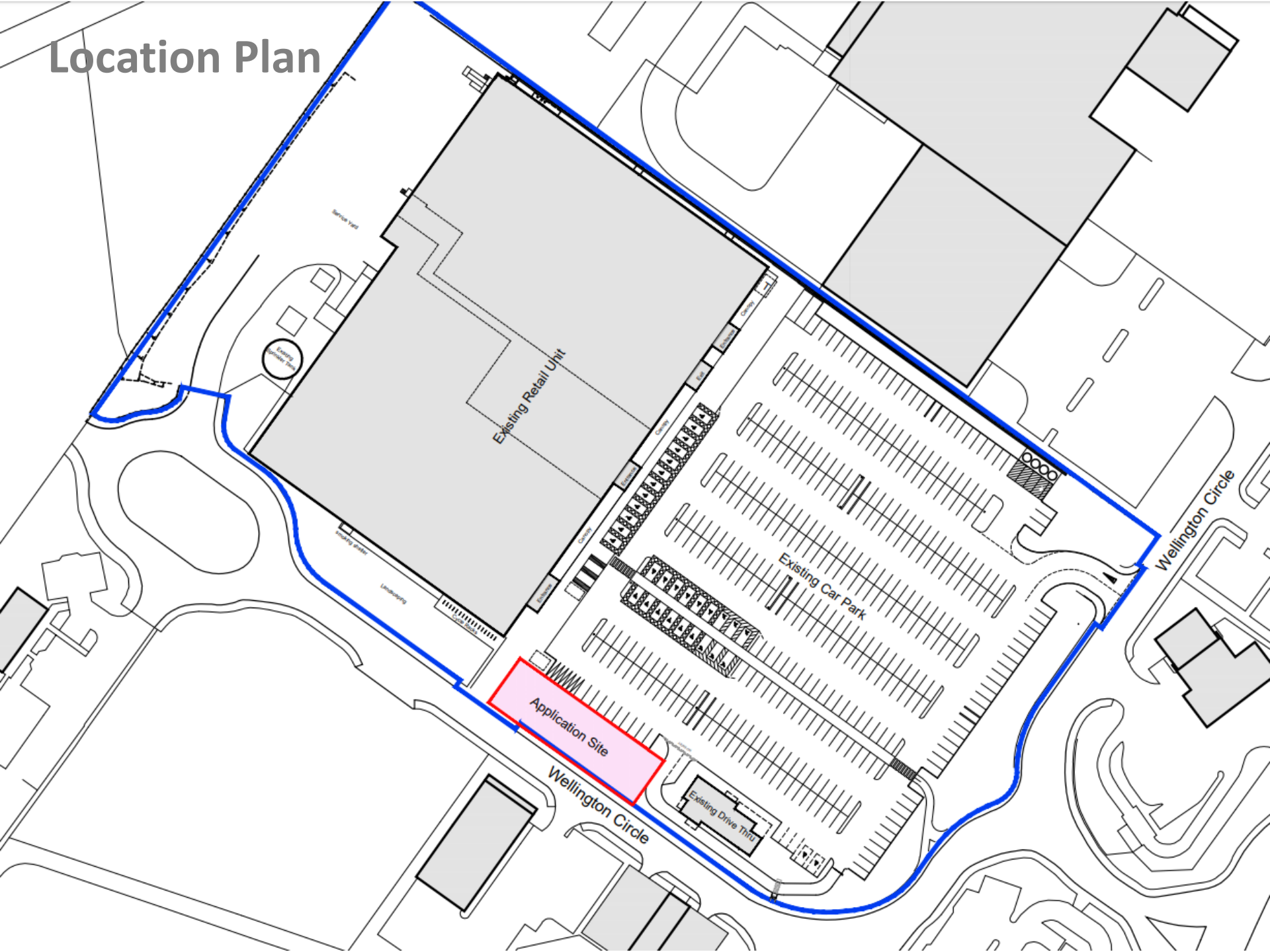
# Location Plan



Denotes Application Boundary.  
Area; 0.064 Ha



# Location Plan



**Location: Aerial Photo**



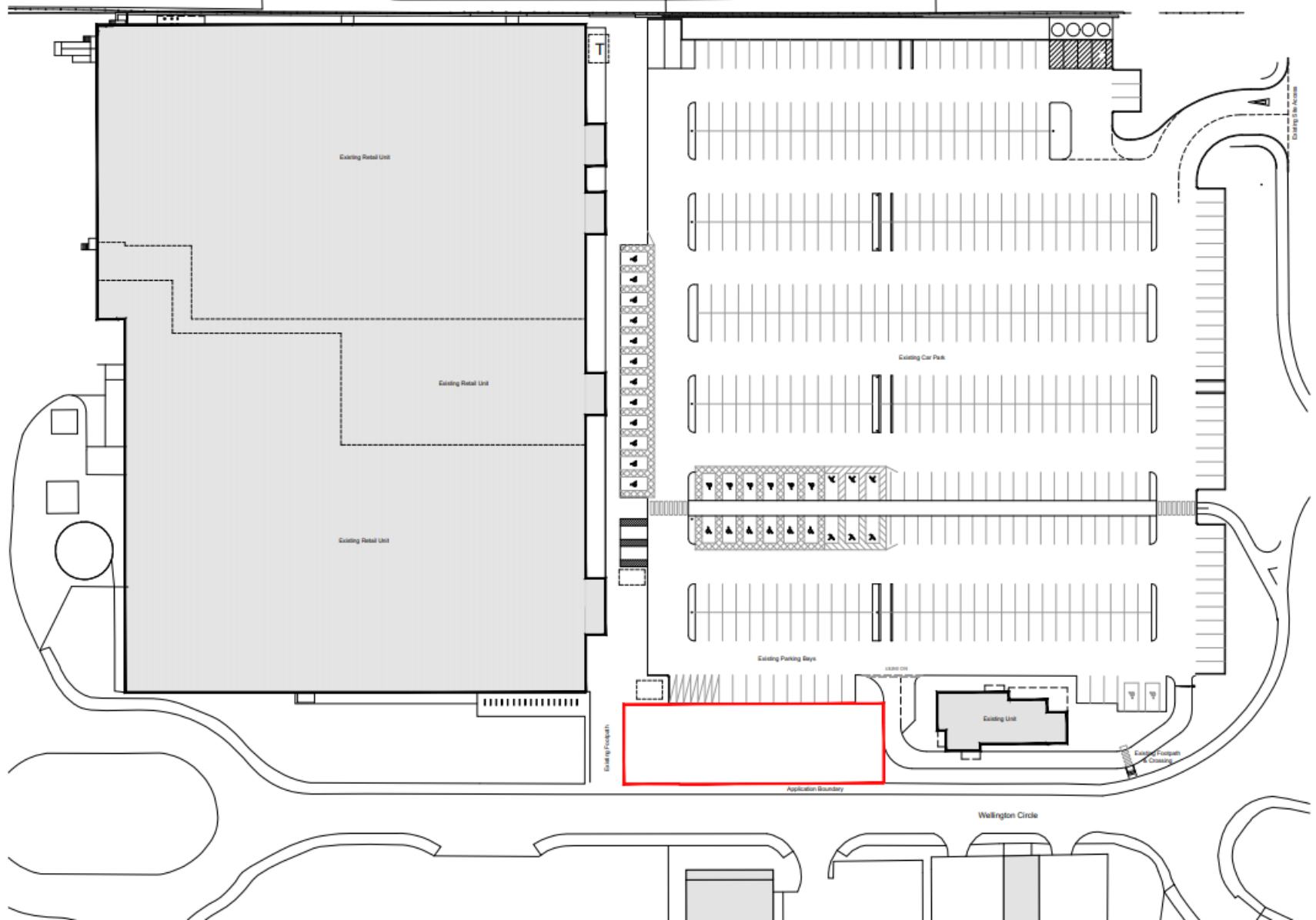
# Street View image (March 2019)



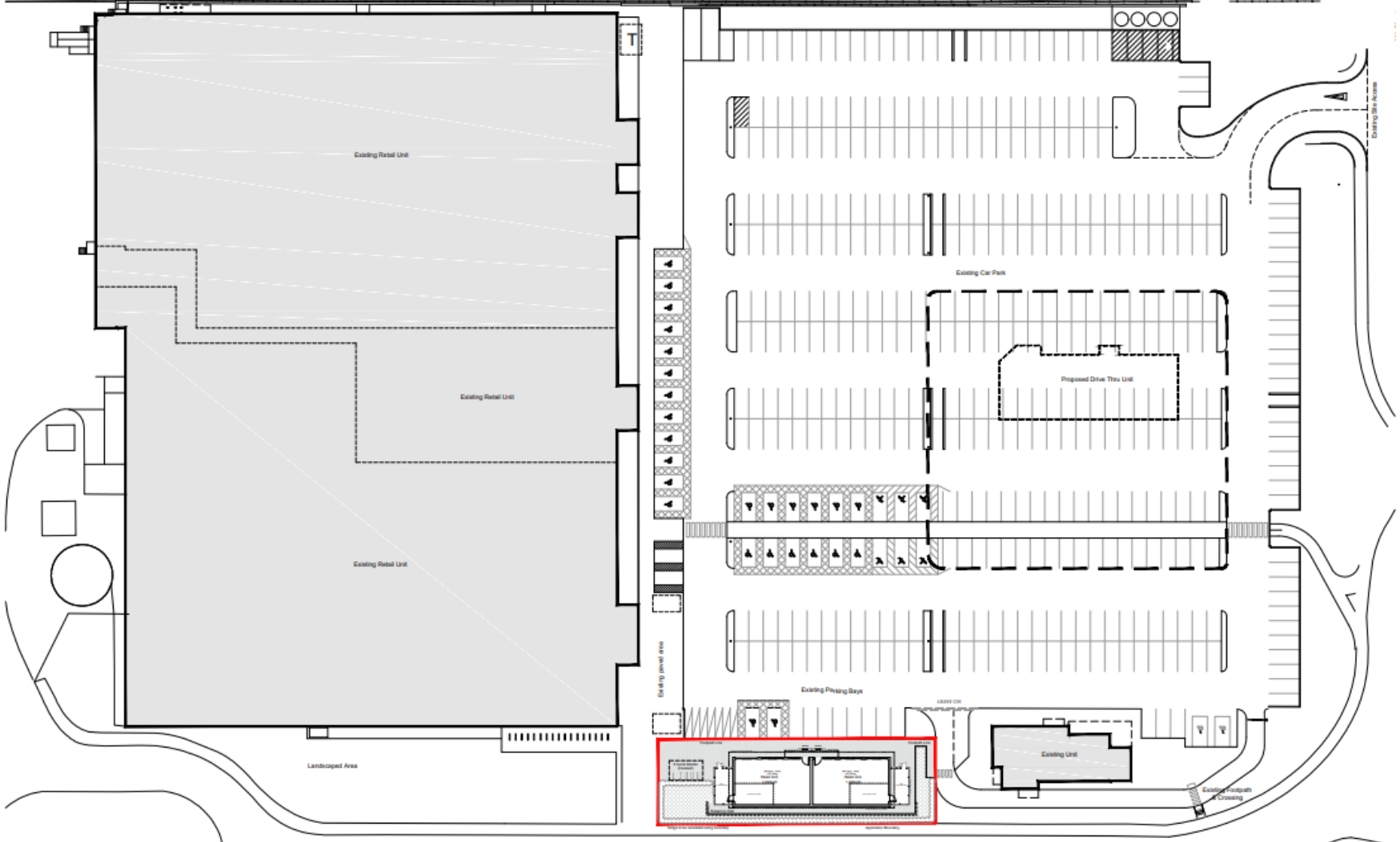
# Wider context: Ikea car park



# Existing Site Plan



# Proposed Site Plan

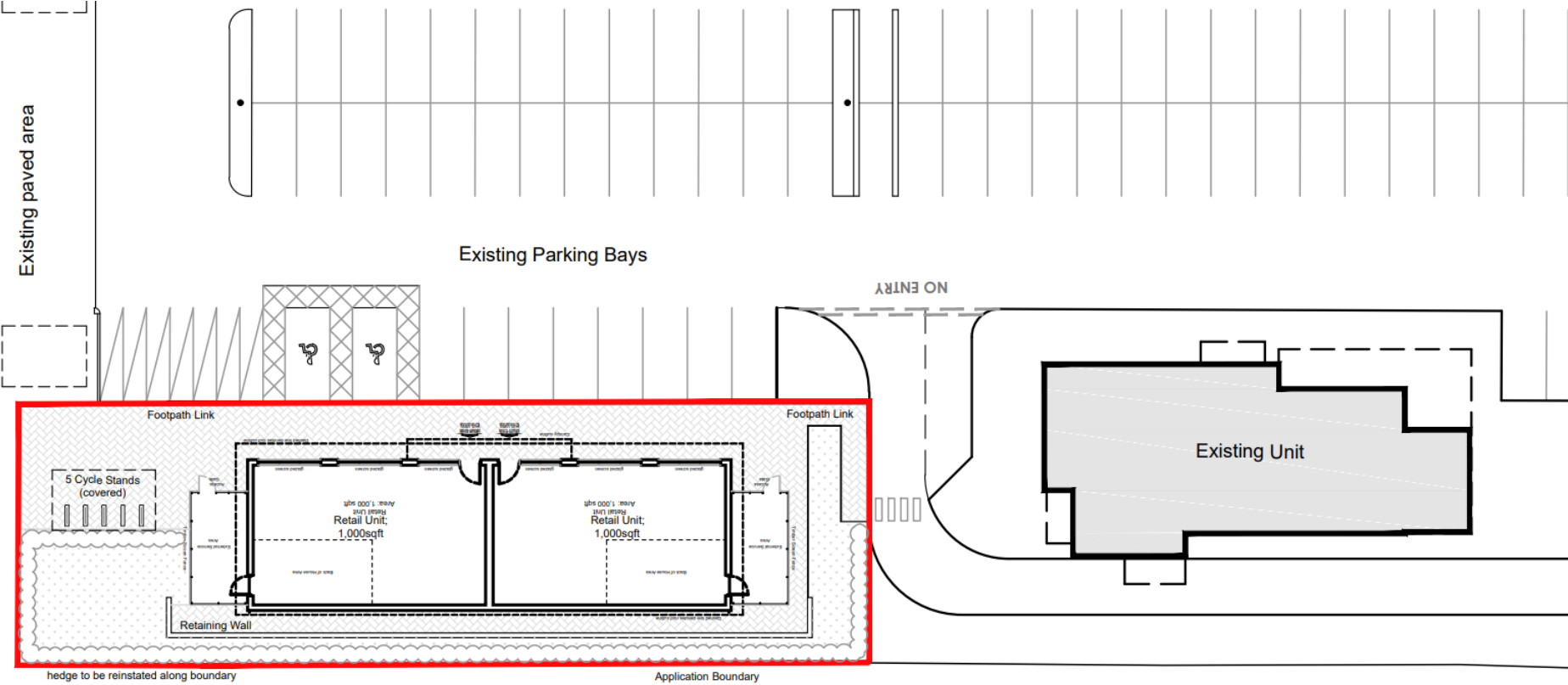


Wellington Circle

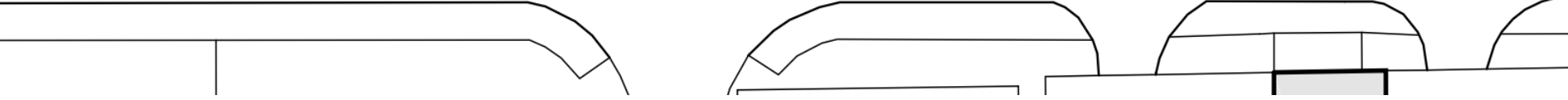
Bunting Hill Avenue



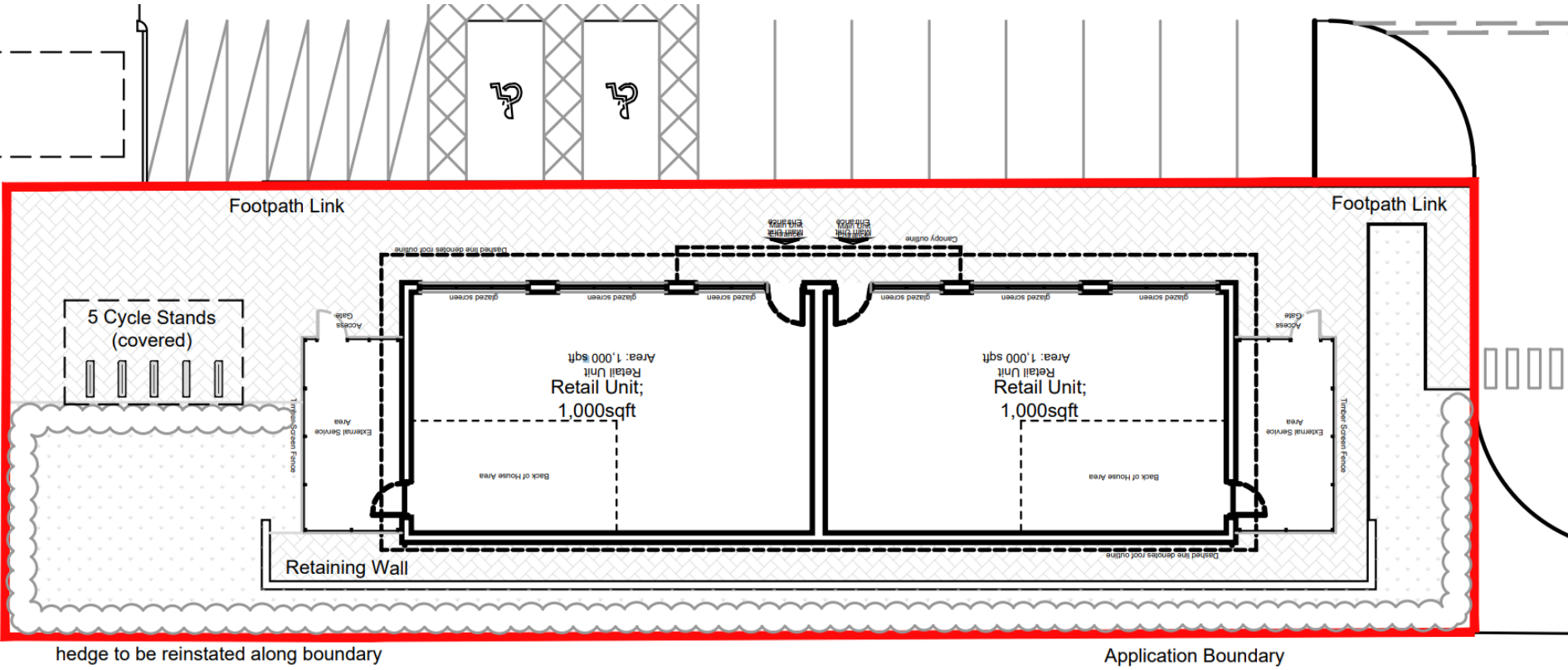
# Proposed Site Plan



Wellington Circle



# Proposed Site Plan



hedge to be reinstated along boundary

Application Boundary

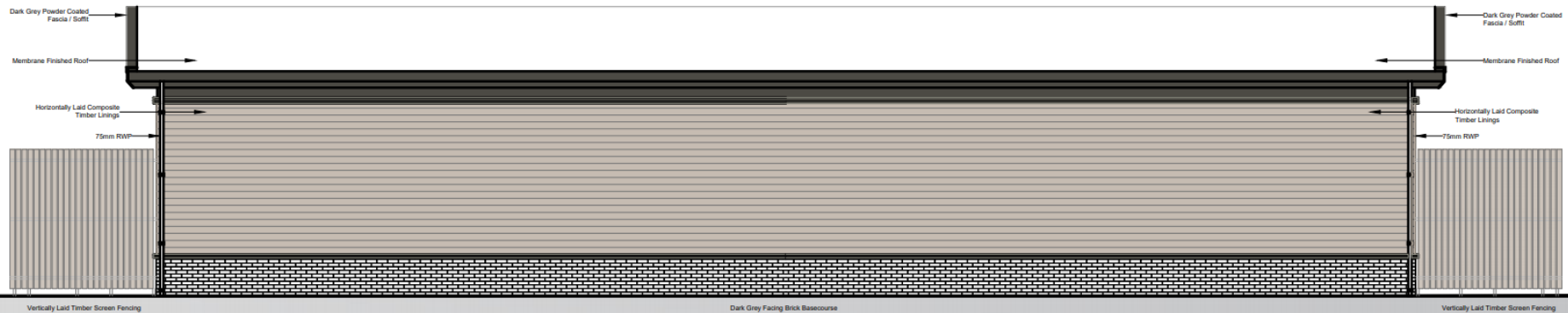
# Proposed North elevation (front – to car park)

- Walls: Horizontal composite timber linings with double glazed screen walling
- Basecourse: Dark grey facing brick
- Fascias/Soffits: Dark grey powder coated
- Windows & doors: Double glazing set within dark grey powder-coated frames
- External service area for each unit enclosed by vertical timber screen fencing



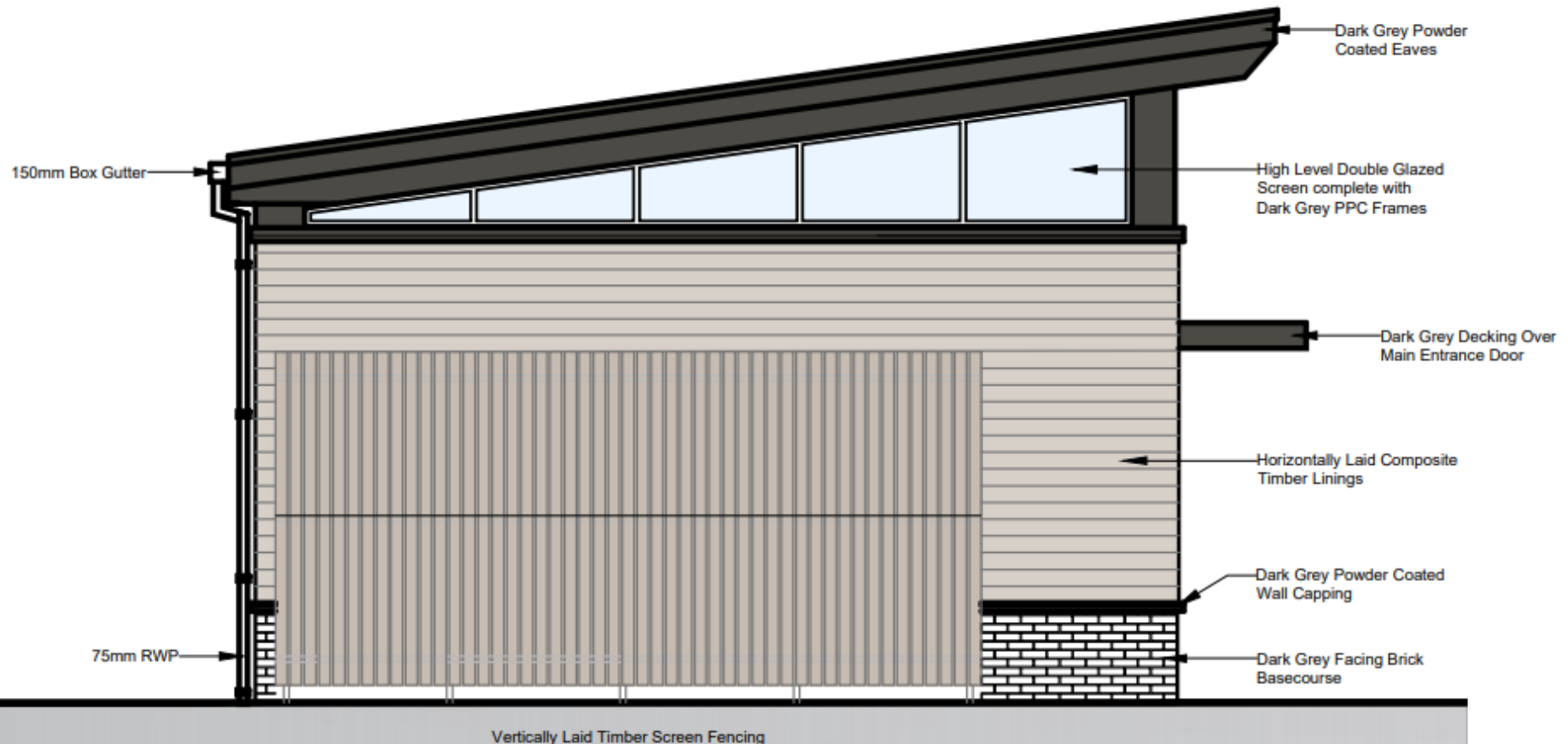
# Proposed South elevation (rear – facing Wellington Circle)

- Walls: Horizontal composite timber linings
- Basecourse: Dark grey facing brick
- Roof: White laminated roof membrane
- Fascias/Soffits: Dark grey powder coated
- External service area for each unit enclosed by vertical timber screen fencing



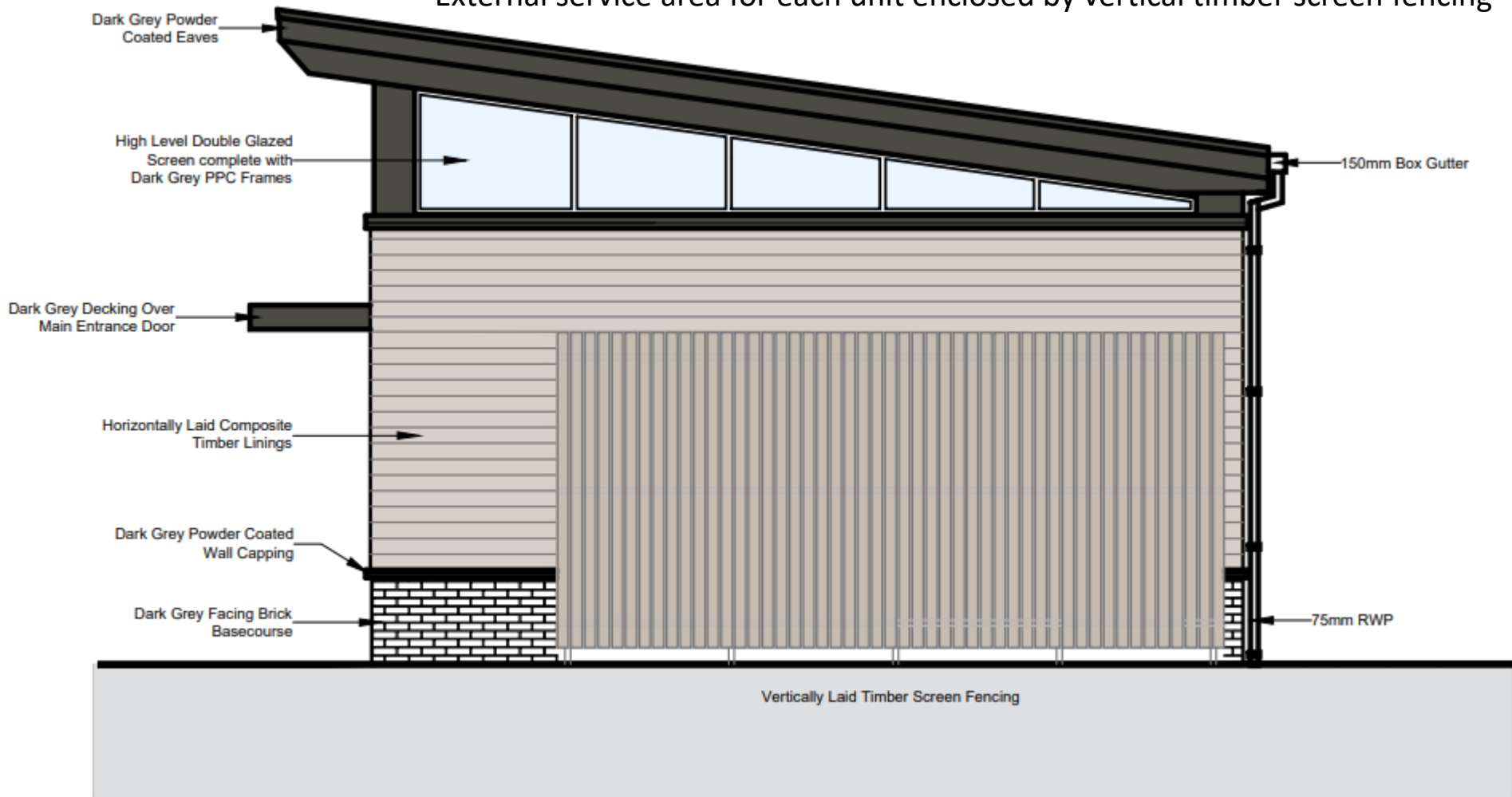
# Proposed West elevation

- Walls: Horizontal composite timber linings with double glazed screen walling
- Basecourse: Dark grey facing brick
- Fascias/Soffits: Dark grey powder coated
- Windows & doors: Double glazing set within dark grey powder-coated frames
- External service area for each unit enclosed by vertical timber screen fencing



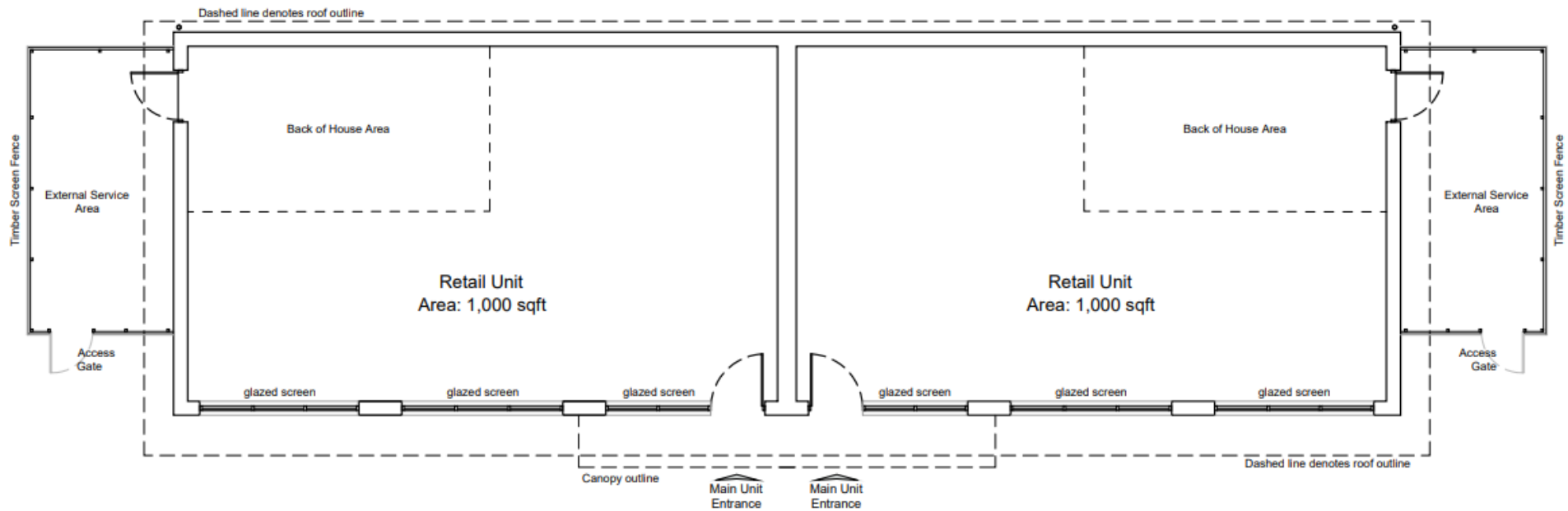
# Proposed East elevation

- Walls: Horizontal composite timber linings with double glazed screen walling
- Basecourse: Dark grey facing brick
- Fascias/Soffits: Dark grey powder coated
- Windows & doors: Double glazing set within dark grey powder-coated frames
- External service area for each unit enclosed by vertical timber screen fencing



# Proposed Ground Floor

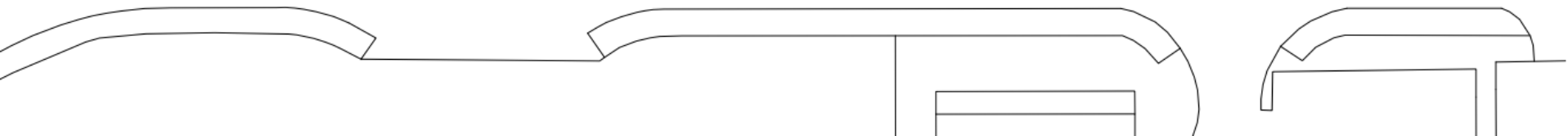
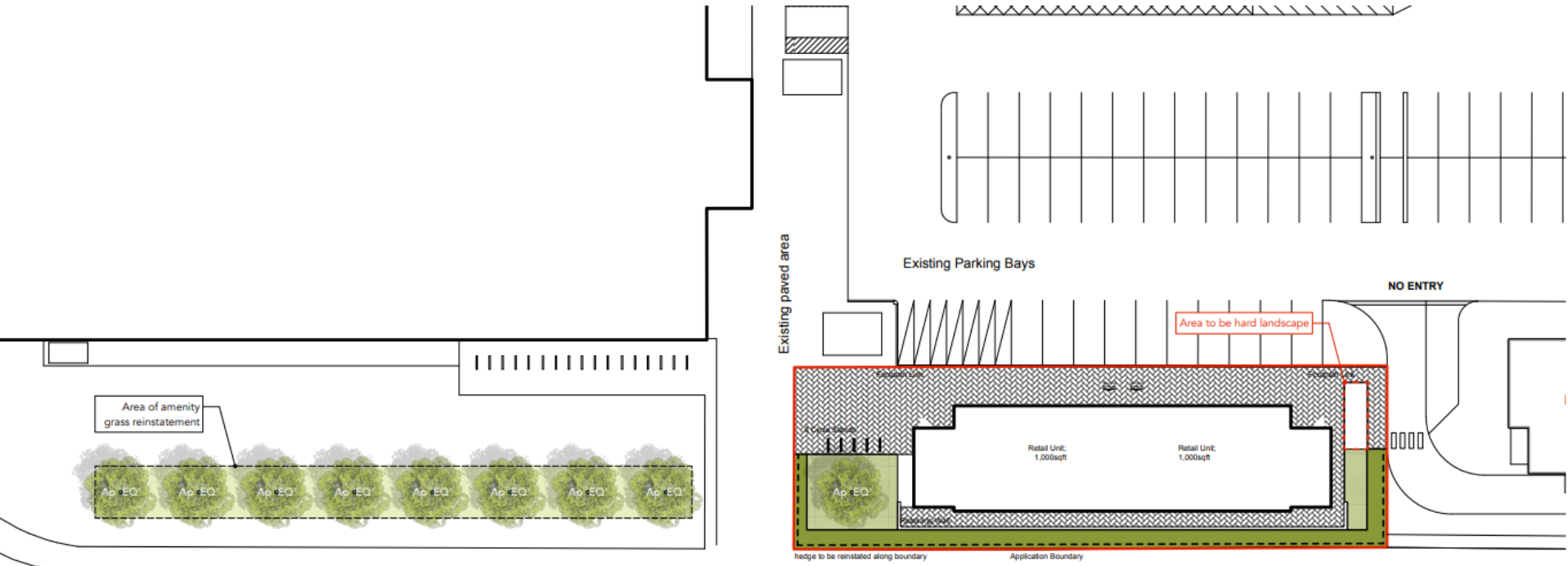
- Each unit 1000sqft, with its own back of house area and external service area
- Roof and entrance canopy shown dashed







# Proposed Landscaping



# Reasons for Decision

In full as part of the agenda pack. Main points are:

- Has not been sited with due consideration for its context, having an uneasy relationship between the existing buildings and that proposed.
- Would result in the loss of a recently re-planted landscaping strip that adds to the character and visual amenity of the surrounding area and is required under planning permission 160067; thereby failing to improve and enhance the setting and visual impact of the proposed development and detrimentally impact on the setting of existing buildings.
- Therefore fails to comply with Policies D1 (Quality Placemaking by Design) and D2 (Landscape of the current Aberdeen Local Development Plan).
- Also fails to comply with corresponding policies in the Proposed Local Development Plan - D1 (Quality Placemaking) and D5 (Landscape Design)
- Principle of development not supported by the Proposed ALDP. However, given that the Proposed ALDP is at the very early stages of consultation on its content, and the site's location within OP110 in the current local plan that still holds significant weight, it is not considered that the conflict with the Proposed LDP would warrant a further reason for refusing the application in this instance.

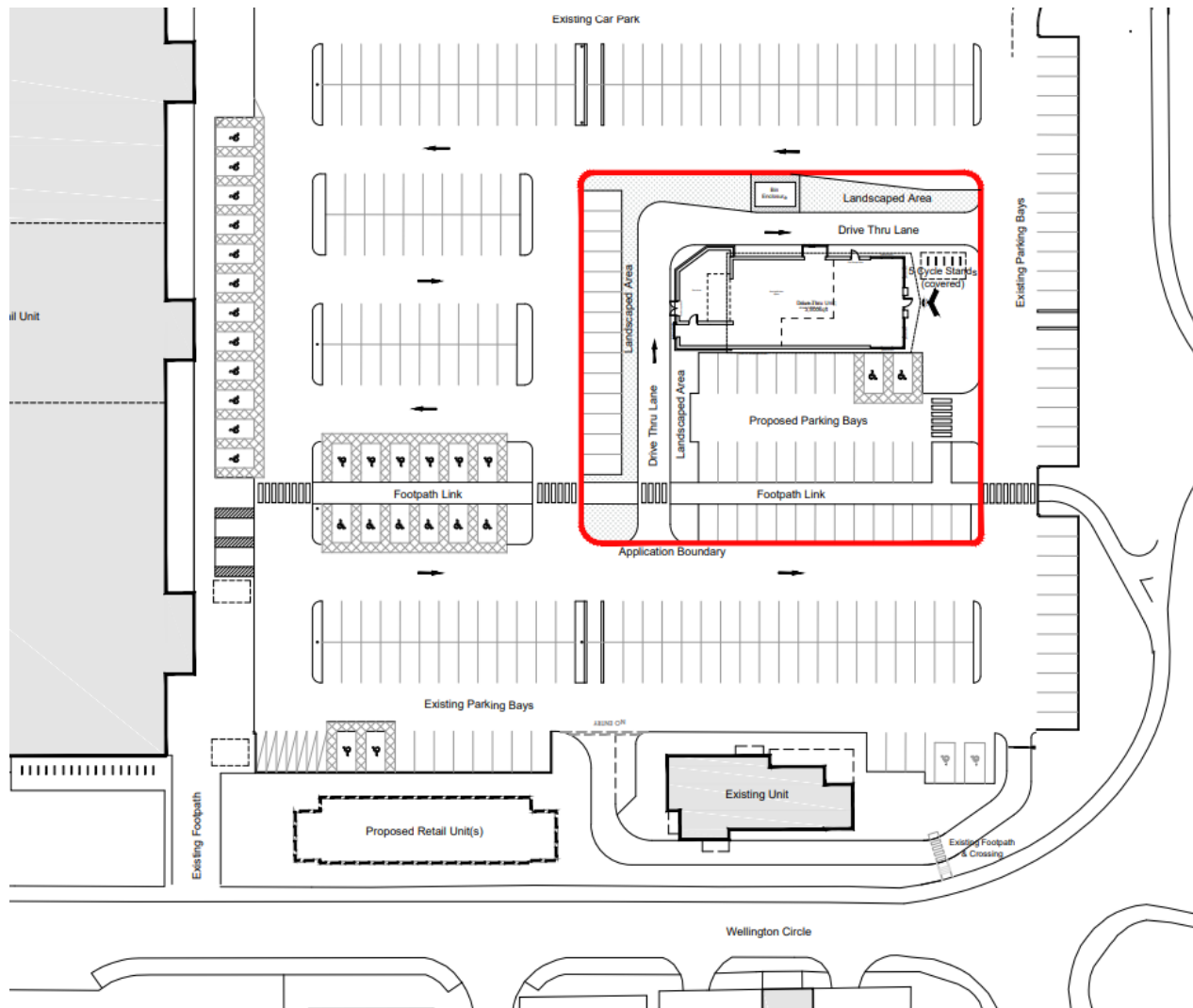
# Relevant Planning History

- 160067 – approval of the neighbouring coffee shop with drive-through facilities (now occupied by Starbucks)



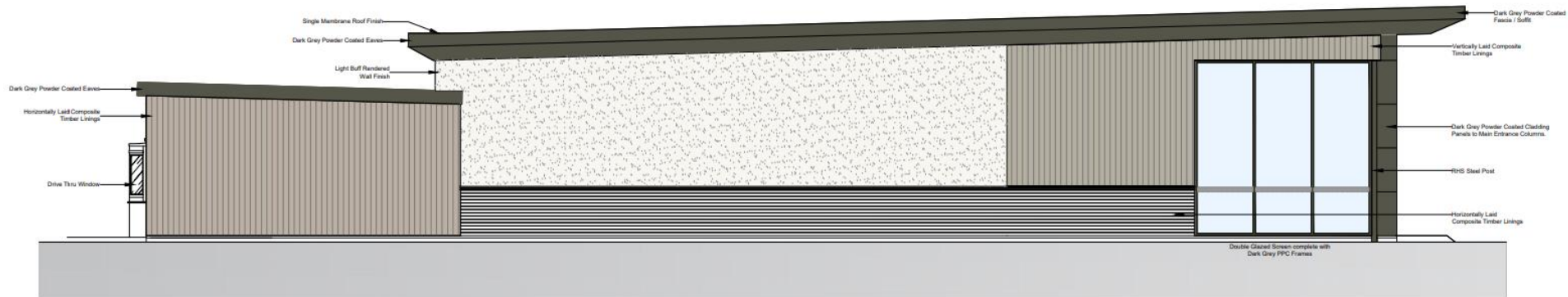
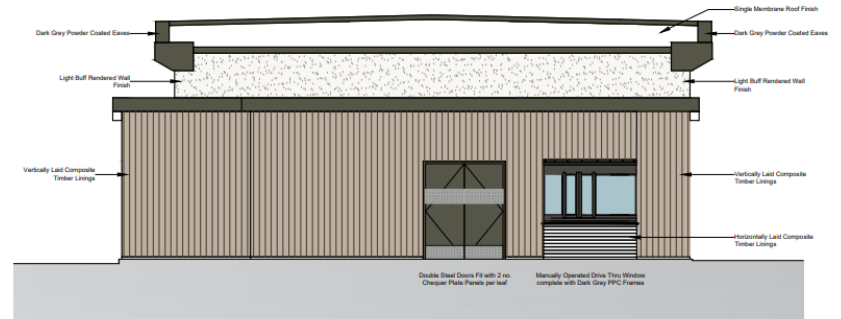
# Relevant Planning History

- 191587 – approval of a restaurant with drive-through facility within the central part of the IKEA car park (approved March 2020)



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- 191587 – approval of a restaurant with drive-through facility within the central part of the IKEA car park (approved March 2020)



# Policy B1 (Business and Industrial Land)

## Policy B1 - Business and Industrial Land

Aberdeen City Council will in principle support the development of the business and industrial land allocations set out in this Plan.

Land zoned for business and industrial uses on the Proposals Map, including already developed land, shall be retained for Class 4 (Business), Class 5 (General Industrial) and Class 6 (Storage and Distribution) uses and safeguarded from other conflicting development types. Other uses which may be suited to a business and industrial location, such as car showrooms and bus depots, shall be treated on their own merits. The expansion of existing concerns and uses within these locations will be permitted in principle.

Where business and industrial areas are located beside residential areas we will restrict new planning permissions to Class 4 (Business). Buffer zones, which are appropriately sized and landscaped, may be required to separate these uses and safeguard residential amenity. Low amenity 'bad neighbour' uses must have regard to surrounding uses and their potential impact on the environment and existing amenity. In all cases, conditions may be imposed restricting levels of noise, hours of operation and external storage.

New business and industrial land proposals shall make provision for areas of recreational and amenity open space, areas of strategic landscaping, areas of wildlife value and footpaths, in accordance with the Council's Open Space Strategy, Supplementary Guidance and any approved planning briefs or masterplans. Within existing business and industrial areas, there shall be a presumption in favour of retaining existing open space.

Facilities that directly support business and industrial uses may be permitted where they enhance the attraction and sustainability of the city's business and industrial land. Such facilities should be aimed primarily at meeting the needs of businesses and employees within the business and industrial area.

- To be retained for uses in classes 4, 5 and 6 (business; general industrial; and storage and distribution)
- Facilities that directly support business and industrial uses may be permitted, where they *'enhance the attraction and sustainability of the city's business and industrial land'*
- Such facilities should be aimed primarily at meeting the needs of businesses and employees within the business and industrial area – would the proposed use serve a much wider catchment?

# Policy B4 (Aberdeen Airport)

- Airport safeguarding map requires consultation with Aberdeen Airport Safeguarding Team
- Proposed developments must not compromise safe operation of the Airport
- Matters such as height of buildings, external lighting, landscaping, bird hazard management and impact on communications/navigation equipment will be taken into account in assessing any potential impact.
- Consultation response from Aberdeen Airport Safeguarding Team states no objection, but draws applicant's attention to best practice on safe use of cranes during construction

# Policy D1 (Quality Placemaking by Design)

## Policy D1 - Quality Placemaking by Design

All development must ensure high standards of design and have a strong and distinctive sense of place which is a result of context appraisal, detailed planning, quality architecture, craftsmanship and materials. Well considered landscaping and a range of transportation opportunities ensuring connectivity are required to be compatible with the scale and character of the developments.

Places that are distinctive and designed with a real understanding of context will sustain and enhance the social, economic, environmental and cultural attractiveness of the city. Proposals will be considered against the following six essential qualities;

- **distinctive**
- **welcoming**
- **safe and pleasant**
- **easy to move around**
- **adaptable**
- **resource efficient**

How a development meets these qualities must be demonstrated in a design strategy whose scope and content will be appropriate with the scale and/or importance of the proposal.

- Does the proposal represent a high standard of design and have strong and distinctive sense of place?



# Policy D2 (Landscape)

## Policy D2 - Landscape

Developments will have a strong landscape framework which improves and enhances the setting and visual impact of the development, unifies urban form, provides shelter, creates local identity and promotes biodiversity. In order to secure high quality development, planning applications for new development must include a landscape strategy and management plan incorporating hard and soft landscaping design specifications. The level of detail required will be appropriate to the scale of the development.

### Quality development will

- be informed by the existing landscape character, topography and existing features to sustain local diversity and distinctiveness, including natural and built features such as existing boundary walls, hedges, copses and other features of interest;
- conserve, enhance or restore existing landscape features and should incorporate them into a spatial landscape design hierarchy that provides structure to the site layout;
- create new landscapes where none exist and where there are few existing features;
- protect and enhance important views of the City's townscape, landmarks and features when seen from busy and important publicly accessible vantage points such as roads, railways, recreation areas and pathways and particularly from the main city approaches;
- provide hard and soft landscape proposals that is appropriate to the scale and character of the overall development.

Further guidance can be found within the Supplementary Guidance and Technical Advice Notes listed in Appendix 5.

# Policy T2 (Managing the Transport Impact of Development)

## Policy T2 - Managing the Transport Impact of Development

Commensurate with the scale and anticipated impact, new developments must demonstrate that sufficient measures have been taken to minimise traffic generated and to maximise opportunities for sustainable and active travel.

Transport Assessments and Travel Plans will be required for developments which exceed the thresholds set out in Supplementary Guidance.

The development of new communities should be accompanied by an increase in local services and employment opportunities that reduce the need to travel and include integrated walking, cycling and public transport infrastructure to ensure that, where travel is necessary, sustainable modes are prioritised. Where sufficient sustainable transport links to and from new developments are not in place, developers will be required to provide such facilities or a suitable contribution towards implementation.

Further information is contained in the relevant Supplementary Guidance which should be read in conjunction with this policy.

# Policy T3 (Sustainable and Active Travel)

## Policy T3 - Sustainable and Active Travel

New developments must be accessible by a range of transport modes, with an emphasis on active and sustainable transport, and the internal layout of developments must prioritise walking, cycling and public transport penetration. Links between residential, employment, recreation and other facilities must be protected or improved for non-motorised transport users, making it quick, convenient and safe for people to travel by walking and cycling.

Street layouts will reflect the principles of Designing Streets and meet the minimum distances to services as set out in the Supplementary Guidance.

Existing access rights, including core paths, rights of way and paths within the wider network will be protected and enhanced. Where development proposals impact on the access network, the principle of the access must be maintained at all times by the developer through provision of suitable alternative routes.

Recognising that there will still be instances in which people will require to travel by car, initiatives such as like car sharing, alternative fuel vehicles and Car Clubs will also be supported where appropriate.

- Emphasis on encouraging active and sustainable travel (e.g. walking, cycling, public transport)
- Need to protect existing links and form new ones where possible
- Scope to also encourage car sharing and low-emissions vehicles, with associated infrastructure

# Policy R6 (Waste Management Requirements for New Development)

## **Policy R6 - Waste Management Requirements for New Development**

All new developments should have sufficient space for the storage of general waste, recyclable materials and compostable wastes where appropriate. Flatted developments will require communal facilities that allow for the separate storage and collection of these materials. Recycling facilities should be provided in all new superstores or large supermarkets and in other developments where appropriate. Details of storage facilities and means of collection must be included as part of a planning application for any development which would generate waste. Further details are set out in Supplementary Guidance.

For proposals where we believe the potential savings on construction or demolition materials for recycling or reuse is likely to be significant, we will ask developers to prepare a Site Waste Management Plan as a condition of planning consent.

# Policy R7 (Low and Zero Carbon Building and Water Efficiency)

## Policy R7 - Low and Zero Carbon Buildings, and Water Efficiency

### Low and Zero Carbon Buildings

All new buildings, must meet at least 20% of the building regulations carbon dioxide emissions reduction target applicable at the time of the application through the installation of low and zero carbon generating technology.. This percentage requirement will be increased as specified in Supplementary Guidance.

This requirement does not apply to:

- 1 Alterations and extensions to buildings;
- 2 Change of use or conversion of buildings;
- 3 Ancillary buildings that are stand-alone having an area less than 50 square meters;
- 4 Buildings which will not be heated or cooled, other than by heating provided solely for the purpose of frost protection; or
- 5 Buildings which have an intended life of less than two years.

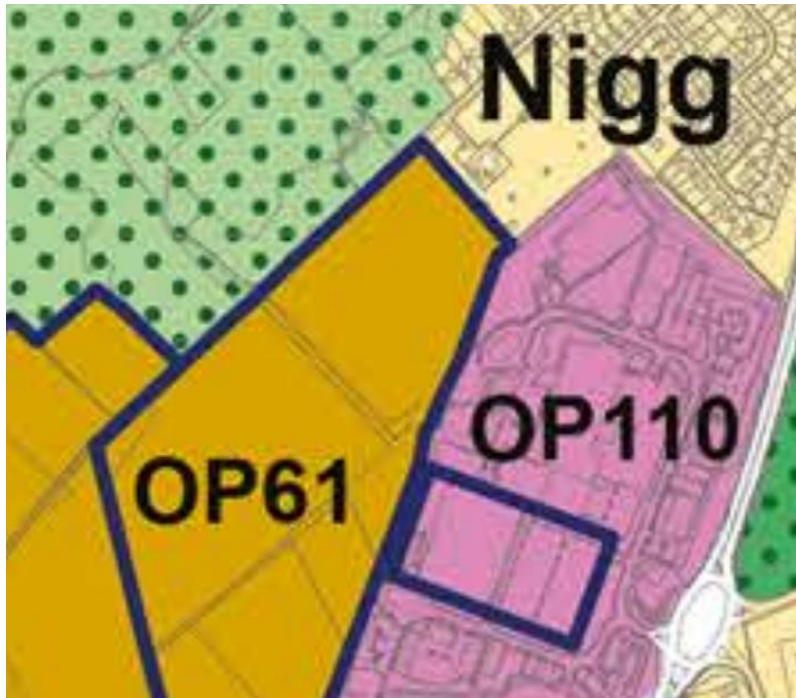
### Water Efficiency

To reduce the pressure on water abstraction from the River Dee, and the pressure on water infrastructure, all new buildings are required to use water saving technologies and techniques. The level of efficiency required and types of efficiencies are detailed in Supplementary Guidance.

Further guidance on compliance with this policy is contained in existing Supplementary Guidance and future Supplementary Guidance on Sustainable Design.

# ALDP Opportunity Site designation

Site	Site Name	Site Size	Policy	Other Factors
OP110	Wellington Circle (Former Makro)	3.2ha	Business and Industrial Land	Opportunity for change of use to Class 1 retail. Drainage Impact Assessment accompanying development proposals should address any surface water flooding issues.



# Points for Consideration:

Zoning: Do members consider that the proposed use is permitted by the terms of policy B1 – i.e. would this development *'enhance the attraction and sustainability of the city's business and industrial land'* and would it cater principally for the needs of the businesses and employees within the business and industrial area (or serve a larger catchment area)?

Opportunity Site designation: The appointed officer considered that the LDP's identification of this site for retail use overrides the Business and Industrial zoning. Do members agree that the principle of retail use in this location is supported by the plan? Do members also agree with the officer's conclusion that the modest floorspace proposed would not warrant consideration of impact on other identified retail centres via sequential testing?

Roads impact: Does the proposal satisfy the terms of policies T2 and T3, which include a requirement that development minimise traffic generated and maximise opportunity for sustainable and active travel?

Design: Is the proposal of sufficient design quality (D1) - *note authorised officer report satisfied on this point.*

**1. Does the proposal comply with the Development Plan when considered as a whole?**

**2. Do other material considerations weigh for or against the proposal? Are they of sufficient weight to overcome any conflict with the Development Plan? Note that OP designation for retail use is not carried forward in Proposed ALDP – what weight does this warrant?**

Decision – state clear reasons for decision

Conditions? (if approved – Planning Adviser can assist)